

Anton Drive

Redevelopment Plan

Public Information Meeting
September 29, 2016

MSA

PROFESSIONAL SERVICES



Tonight's Topics

- Schedule and Process
- Major Themes
- Streets and Traffic
- Walking and Biking
- Community Meeting Spaces
- Land Use and Development
- Urban Design



Schedule and Process

- Committee Meetings
 - September 14
 - October 26-27 (tent.)
 - November 29-30 (tent.)
- Plan Commission Review
 - September 20
- Public Information Meeting
 - Tonight
- Public Hearing/Plan Commission Recommendation
 - January 17
- Council Adoption
 - February 28

Major Themes

- This area will be unique, urban and walkable
- New development will be compatible with and support the success of the Jamestown Neighborhood
- Intersections will safely accommodate traffic as development occurs

Streets and Traffic

- New streets in quarry area as proposed by developers
- No new street connections to Jamestown Neighborhood proposed
- Limits on new development and trip generation to ensure continued function of intersections
- Future changes to intersections
 - Fitchrona at McKee – second left turn lane, southbound onto McKee
 - Kapec at Fitchrona – right turn lane
 - King James Way at Anton – roundabout option
 - Consider changes to Whitney Way and Williamsburg (per public comments)

Walking and Biking

- Sidewalk on both sides of all streets within planning area as development occurs
- Multi-use off-street path on west side of Anton Dr., Fitchrona Rd.
- Paved path through conservancy land at west end of quarry
- Paved path connections to Jamestown Neighborhood
 - King James Court
 - Chesapeake Dr./Danville Drive

Community Meeting Spaces

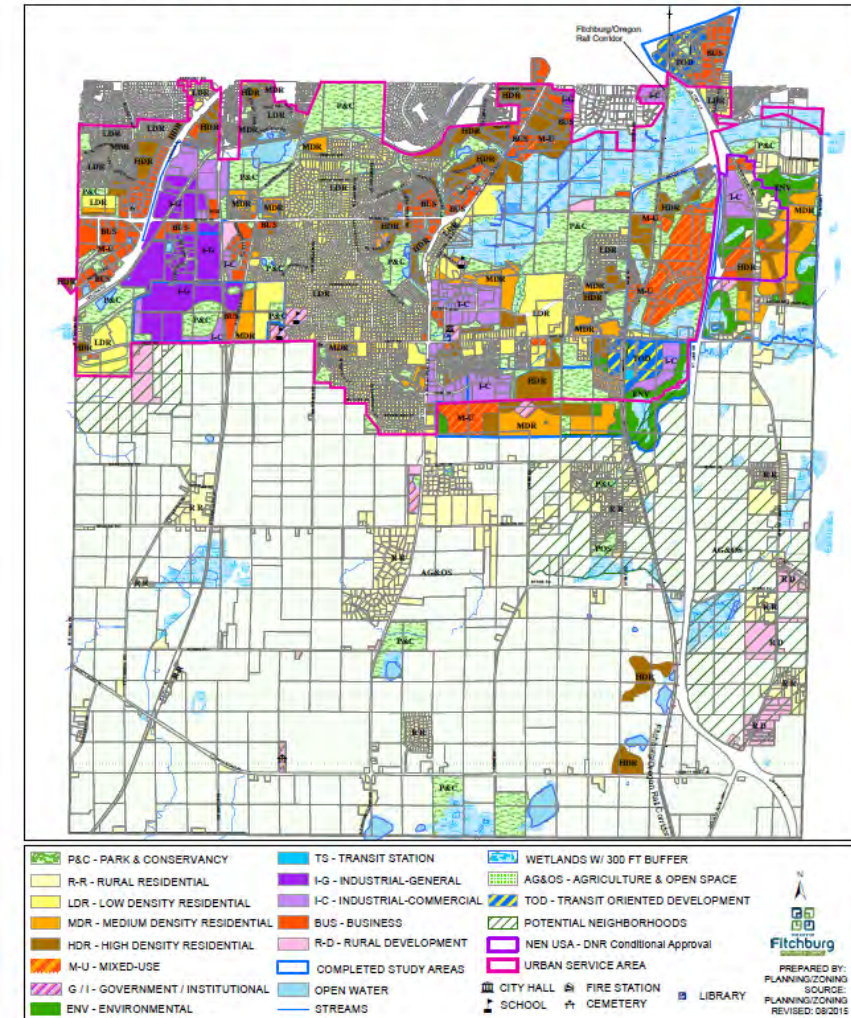
- Fire Station #2 planned for redevelopment
- No public community center or meeting spaces proposed in this plan, but possible in the future
- Private meeting spaces in new development available for public use are encouraged; City will consider funding assistance to assure public access to such spaces

Land Use and Development

- The City will amend the Future Land Use Map when adopting this plan

Figure 4 - 10

FUTURE LAND USE PLAN MAP



Land Use and Development

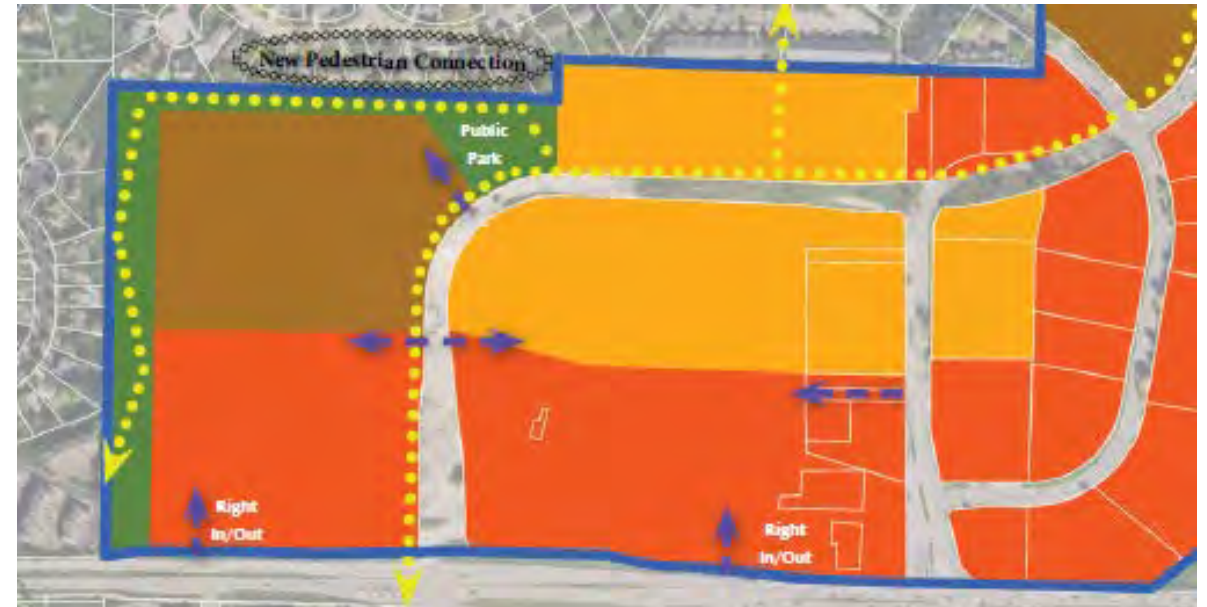
- The best uses (based on market demand and location) are commercial and residential
- Industrial/manufacturing/storage is not desirable in this location
- Office use is desirable, but demand is limited
- Civic uses (e.g. a church or community center) are not specifically planned, but are compatible with residential use, and a good transition use.

Land Use and Development

- About new residential uses:
 - Diverse housing types are desired – rental, owner-occupied, smaller, larger, senior housing, etc.
 - Affordability is an important City-wide issue, but not a high priority in this neighborhood at this time. Market-rate affordability is encouraged. New income-qualified units are not preferred at this time. There are about 116 such units in New Fountains right now – considering a policy to maintain that number in the neighborhood.

Land Use and Development

- The development illustrations (left) show what could be, but they are not public policy. They inform our edits to the Future Land Use Map (right), which is public policy.



Med Density Residential



High Density Residential



Business



Mixed Use



CONCEPT "A"

Land Use Policy Map

ANTON DRIVE

REDEVELOPMENT PLAN

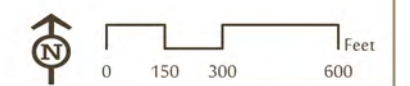
Future Land Use

Proposed by Anton Drive Redevelopment Plan

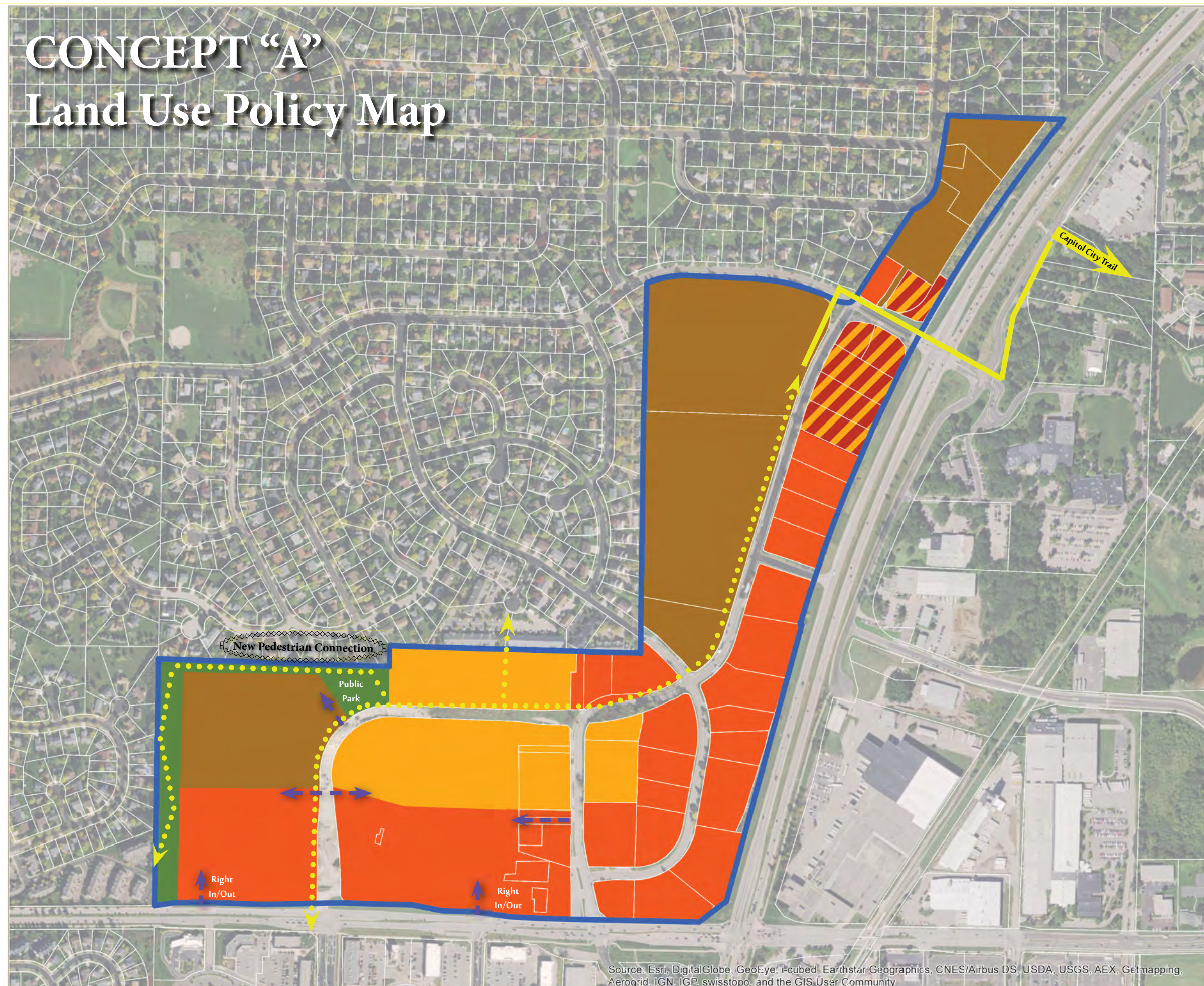
-  Planning Area
 -  Parcel
- Future Land Use**
-  Low Density Residential
 -  Medium Density Residential
 -  High Density Residential
 -  Mixed Use
 -  Business
 -  Industrial - Commercial
 -  Industrial - General
 -  Park & Conservancy
- Potential Access/Trail**
-  Pedestrian Access/Trail
 -  Vehicle Access

DATA SOURCES:
 PARCELS AND ROAD NAMES PROVIDED BY DANE COUNTY.
 FUTURE LAND USE PROVIDED BY THE CITIES OF FITCHBURG AND MADISON.
 AERIAL IMAGERY PROVIDED BY ESRI.

CITY OF FITCHBURG
 DANE COUNTY, WI



Source: Esri, DigitalGlobe, GeoEye, i-cubed, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community



CONCEPT "A" Development Illustration

This is a conceptual illustration of potential development in the Anton Drive planning area, provided to support discussion about how this area could develop and the drafting of land use and urban design policy for the area. While the final plan document will include a conceptual plan in this format, showing specific streets, parcels and buildings, all participants should understand that development will be proposed by property owners, and subject to City review and approval using this plan as guidance for that approval.



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Development Illustration

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RESIDENTIAL UNIT SUMMARY	Existing	Additional Illustrated	Sub Total
Mid-Rise Multi-Unit (4+ stories)	0	48	48
Low-Rise Multi-Unit (Less than 3 stories)	665	111	776
Fourplex Buildings	0	24	24
Rowhousing	16	33	49
Bungalow Court Homes	0	63	0
TOTAL UNITS	681	279	960

FIRST PRIORITY REDEVELOPMENT

SECOND PRIORITY REDEVELOPMENT

FIRST PRIORITY REDEVELOPMENT

SECOND PRIORITY REDEVELOPMENT

63 Res. Units

4-STORY MULTI-UNIT
36 UNITS

3-STORY MULTI-UNIT
27 UNITS

2-STORY OFFICE
35,000 SF

2-STORY OFFICE
19,300 SF

3-STORY OFFICE
24,525 SF

3-STORY OFFICE
19,800 SF

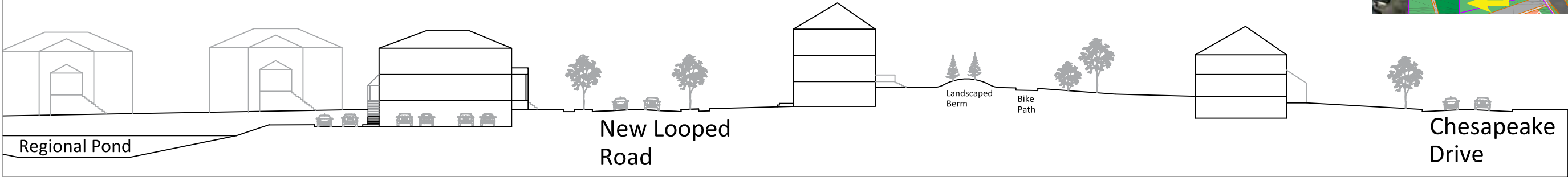
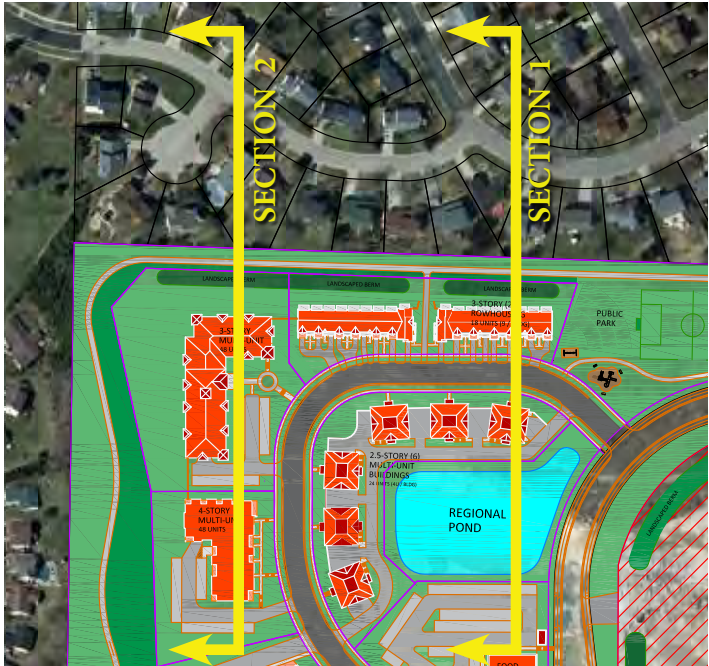
3-STORY MIXED COM
48,000 SF

3-Story Mixed Com
10,600 SF Retail
20,500 SF Office

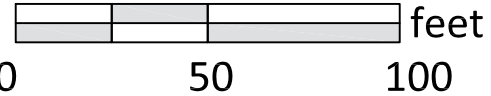
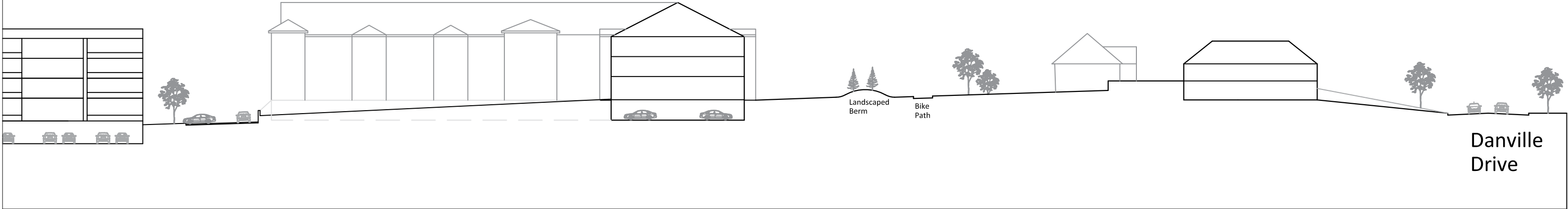
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CONCEPT "A"

Section 1



Section 2



CONCEPT "B"







Land Use Policy Map



ANTON DRIVE

REDEVELOPMENT PLAN

Future Land Use

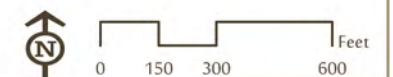
Proposed by Anton Drive Redevelopment Plan

-  Planning Area
 -  Parcel
- Future Land Use**
-  Low Density Residential
 -  Medium Density Residential
 -  High Density Residential
 -  Mixed Use
 -  Business
 -  Industrial - Commercial
 -  Industrial - General
 -  Park & Conservancy

- Potential Access/Trail**
-  Pedestrian Access/Trail
 -  Vehicle Access

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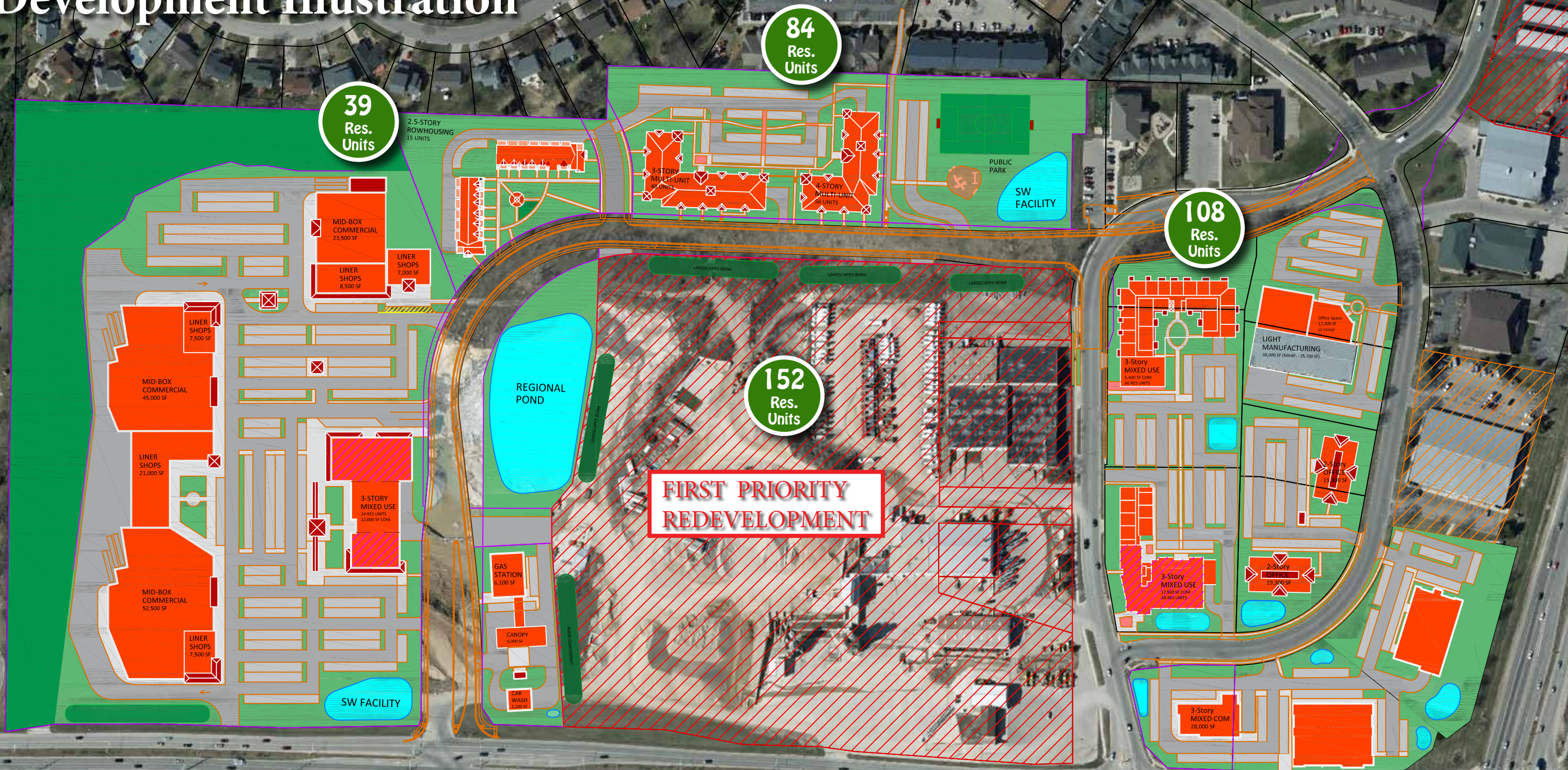
CITY OF FITCHBURG
 DANE COUNTY, WI



Source: Esri, DigitalGlobe, GeoEye, i-cubed, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community

CONCEPT "B" Development Illustration

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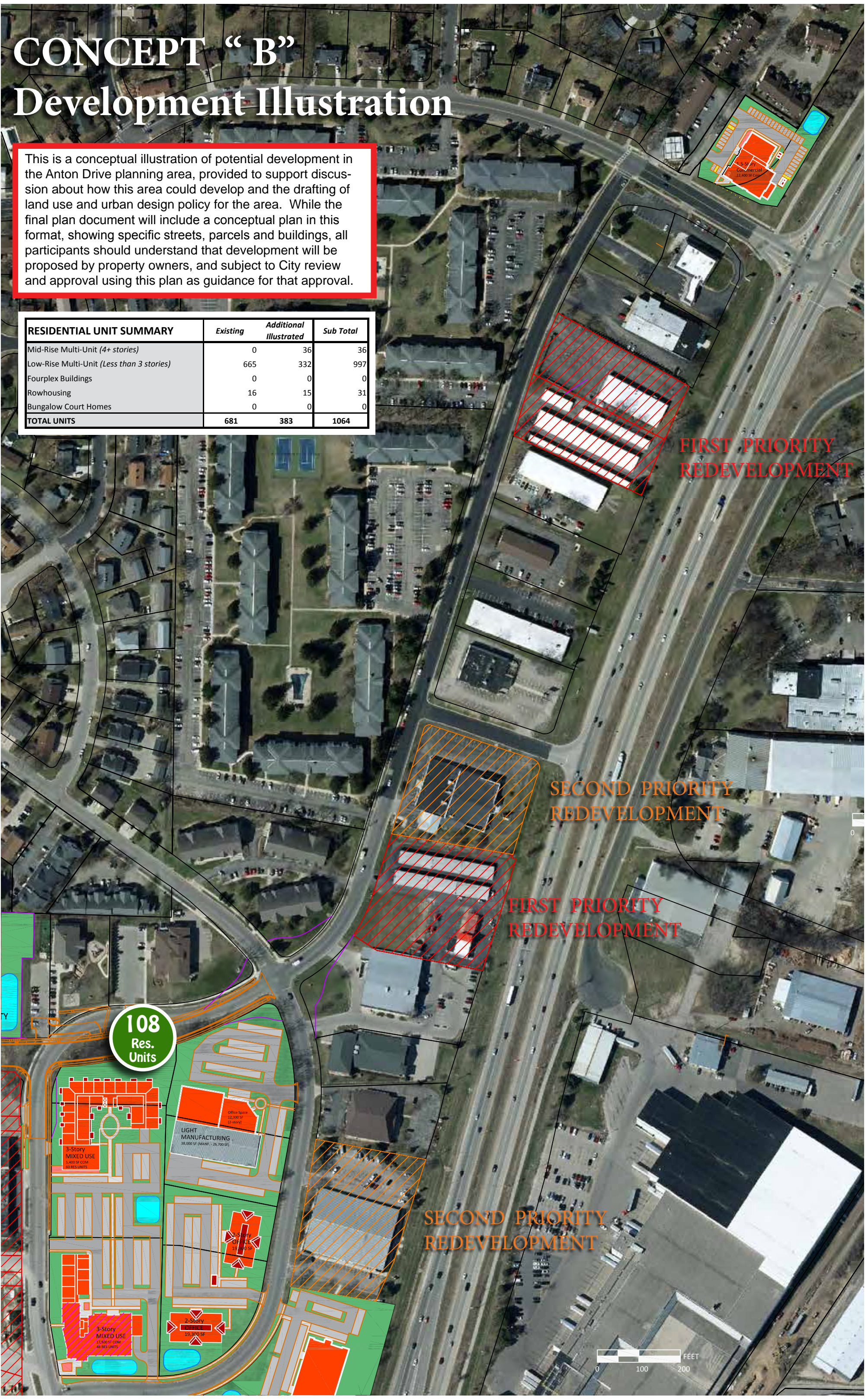


CONCEPT "B"

Development Illustration

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RESIDENTIAL UNIT SUMMARY	Existing	Additional Illustrated	Sub Total
Mid-Rise Multi-Unit (4+ stories)	0	36	36
Low-Rise Multi-Unit (Less than 3 stories)	665	332	997
Fourplex Buildings	0	0	0
Rowhousing	16	15	31
Bungalow Court Homes	0	0	0
TOTAL UNITS	681	383	1064



FIRST PRIORITY REDEVELOPMENT

SECOND PRIORITY REDEVELOPMENT

FIRST PRIORITY REDEVELOPMENT

SECOND PRIORITY REDEVELOPMENT

108 Res. Units

3-Story MIXED USE
5,400 SF COM
60 RES UNITS

Office Space
12,300 SF
(2-story)

LIGHT MANUFACTURING
38,000 SF (MANF. - 25,700 SF)

3-Story OFFICE
19,200 SF

2-Story OFFICE
19,200 SF

3-Story MIXED USE
5,400 SF COM
60 RES UNITS






This plan proposes that a portion of the Anton Drive Planning Area should be designated as G4 – Infill Growth Sector. If adopted as part of this plan, the City’s Sector Plan will be amended to reflect this designation, enabling owners of these lands the option of utilizing SmartCode Zoning.

ANTON DRIVE REDEVELOPMENT PLAN

SECTOR PLAN

Proposed by Anton Drive Redevelopment Plan

-  Municipal Boundary
-  Planning Area
-  Parcel

Open Space and Growth Sectors

-  01 - Preserved Open Sector
-  02 - Reserved Open Sector
-  G2 - Controlled Growth Sector
-  G3 - Intended Growth Sector
-  G4 - Infill Growth Sector
-  G5 - Repair Sector

Potential Access/Trail

-  Pedestrian Access/Trail

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This is a sample regulating plan, an example of how the lands designated for optional SmartCode zoning could be assigned to transects within the G4 Sector. While the final plan document will include a sample regulating plan in this format, showing specific streets, blocks and transects, all participants should understand that development will be proposed by property owners, and subject to City review and approval. Those proposals may or may not utilize SmartCode zoning within this area, and, if utilizing SmartCode, the regulating plan may differ from this sample, with a different configuration of T3, T4 and T5 transects as permitted in the G4 Sector. Note that if SmartCode Zoning is proposed, the land use designations that guide conventional zoning approvals will not apply.

ANTON DRIVE REDEVELOPMENT PLAN

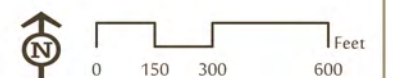
REGULATING PLAN

Proposed by Anton Drive
Redevelopment Plan

-  Municipal Boundary
-  Planning Area
-  Parcel
-  T1 Natural Zone
-  T4 General Urban Zone
-  T5 Urban Center Zone
-  Stormwater Management Facility
-  Civic Zone
-  Playground

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T4 (General Urban)
Residential



T4 (General Urban)
Commercial / Mixed Use



T5 (Urban Center)
Residential



T5 (Urban Center)
Commercial / Mixed Use



Urban Design

- Verona Road lots (double frontage):
 - Building entrances should face the local street
 - Multistory buildings and elevated roof forms encouraged, especially where the highway is elevated. No visible rooftop mechanicals
 - Loading docks and garage doors in the side yards to minimize view from either roadway

Urban Design

- All areas:
 - Multistory buildings strongly encouraged
 - Shallow building setbacks, no front-yard parking
 - Monument signs and building-mounted signs preferred
 - Full cutoff lighting
 - Welcoming entrances with safe connections to the public sidewalk

ANTON DRIVE PROPOSED DESIGN GUIDELINES

Site Design & Street Relationship

INTENT: To establish a neighborhood that is urban in character and safe and pleasant for pedestrians.

Recommendations

A. Buildings should be built close to the street, with no front-yard parking, whenever possible. This should apply to all building types and uses, without exception.

B. Building setbacks should be uniform along a street to establish a consistent “street wall”.

In locations where a street curves or ends, buildings should be sited and designed either to establish a strong visual “terminus” to the view down the street, or alternatively, to preserve a vista of open space or design landscape feature.

C. All buildings should have an active, public entrance facing at least one public street. In most cases this should be the primary building entrance.

D. Building entrances should be connected to the public sidewalk by an attractive and accessible path/walkway.

E. All sites should incorporate usable outdoor spaces. These can be public, private with public access, or private, depending on the building use. All such spaces should be designed with a clear connection to the building use and interior spaces, to make them safe and active.

F. For sites abutting Verona Road, the preceding guidelines should be applied to focus buildings toward the local street, not toward the highway.



The ADA ramp is incorporated in the stair entrance and meets the needs of all users.



An example of a public space that provides outdoor space for customers, employees, residents and the general public.

Parking, Access and Stormwater Management

INTENT: To provide parking lots that are safe for drivers and pedestrians, while mitigating the visual and environmental impacts of surface parking.

Recommendations

A. Parking should be located in the side yard and rear yard, or beneath buildings.

B. Parking should be coordinated across adjacent sites. Shared parking arrangements are encouraged. Adjoining retail/service uses should incorporate pedestrian and vehicle connections so that customers can get from one business to another without using the public street.

C. Bicycle parking facilities are strongly encouraged.

D. Parking lots with rows of more than twenty (20) parking spaces should be interrupted by a landscape median or island.

E. A buffer should be provided between parking lots and adjoining streets using landscaping and/or decorative wall/fencing. A height of two to four feet is recommended, to partially hide vehicles but maintain some visual connection between parking areas and the street.

F. Parking lots should be landscaped along their edges and within each parking island. The incorporation of required stormwater detention and infiltration devices into the design of the parking area is encouraged.

G. Pedestrian walkways should be provided in parking areas to allow safe access to building entrances.



This illustration shows two developments on adjoining lots sharing parking and an access drive. A sidewalk connects the two developments through the parking area.



This illustration shows a median and parking islands breaking up the parking stalls. Also the parking stalls are separated from the public sidewalk by a landscaping divider.



Examples of desired stormwater management designs within parking lots.



ANTON DRIVE PROPOSED DESIGN GUIDELINES

Signage

INTENT: To promote effective and attractive signage appropriate to an urban area.



Examples of preferred signage.

Recommendations

- A. Preferred sign types include building-mounted and free-standing monument signs.
- B. Site signage should be coordinated with buildings, in color, material, and design.
- C. A concealed light source is the preferred method for lighting monument signs. The use of small, well-designed light fixtures is the preferred method for illuminating building mounted signage.
- D. Billboards, roof top signs, and pole signs are discouraged.
- E. Free standing signs should be coordinated with landscaping and other features to identify site entrances and maintain a strong “edge” along the public street.

Lighting

INTENT: To promote effective and attractive exterior lighting that does not produce glare or light pollution.

Recommendations

- A.. Exterior lights should use “full cutoff” luminaires that light building, parking and walkway surfaces but do not shine outward into the eyes of drivers or neighboring residents.
- B. Whenever possible, architectural, landscape and sign lighting should be mounted above the lighted surface and directed toward the ground, to limit light pollution and sky glow.
- C. Exterior light fixtures should be coordinated with the overall site and building design.



Examples of full cutoff fixtures that minimize glare and light pollution



An example of a shielded light fixture that cuts down on light trespass.

ANTON DRIVE PROPOSED DESIGN GUIDELINES

Building Design

INTENT: To establish urban character, including streets and places that are safe and pleasant for walking.



This building has architectural details that break down the facade to human scale and establish vertical proportions along a wide facade.



An example of a well-defined base, body, and cap.



This example shows the use of a recessed entry to identify its location, and changes in material and wall plane to break up the side facade.

Recommendations

A. True multi-story buildings are strongly encouraged, especially adjacent to Verona Road, McKee Road, and Fitchrona Road. For one- or two-story buildings, accentuate the building height with elevated roof forms and clerestory designs that add height, preferably in a functional way by enhancing the interior space as well.

B. Pitched roofs should have a slope no less than 5/12. Roof designs should be appropriate to the size of the building; larger buildings should generally have flat roofs with parapet walls. In no case should a new building near Verona Road have a flat roof visible from the highway; this can be avoided with taller buildings or, if necessary, the use of a false roof or extended parapet wall around the out edges of a flat roof to hide the rooftop and any mechanical systems.

C. Vertical proportions are strongly encouraged, and long, undifferentiated walls and rooflines are strongly discouraged. When a building is wider than it is tall, consider the use of varied roof forms, facade projections, window patterns, materials and other architectural features to emphasize vertical proportions.

D. All buildings should establish a base, body and cap. The base and the cap should be clearly distinguishable from the body through changes in color, material, and/or profile.

E. Buildings should have details and proportions along all street and parking lot facades that are scaled to the pedestrian. This can generally be achieved through architectural detailing and the use of windows, awnings, canopies and lighting, especially at the first floor level.

F. All buildings should have clearly-defined and welcoming entrances. The use of awnings, canopies, porticos, covered porches and other techniques to provide shelter outside the doorway are strongly encouraged.

Building Design

Recommendations

G. All sides of the building should include materials and design characteristics consistent with the front façade. Use of lesser quality materials for the sides and rear facades should be minimized.

All service, refuse, garage doors and loading dock areas should be screened from public view through strategic placement, landscaping, and/or architectural design integration. For sites with dual frontage configurations, such as those along Verona Road, these features should generally be located along a side yard, and not prominently visible from either the highway or the local street.

I. While all buildings should be close to the street, most residential buildings should use a first floor elevation at least three feet higher than the adjacent public sidewalk to maintain comfort and privacy for residents. Look for opportunities to use grade changes across the site to also provide accessible entrances to the building.



Single family homes with porches and raised first-floor elevations are both welcoming and sufficiently private while close to the street.



Architectural details add visual interest and vertical proportions to a wide building



Various techniques in use to break down the apparent mass of a large building, including canopies, recessed decks, recessed top story, and variations in materials and wall plane.

Providing Feedback

- Talk to a member of the Steering Committee tonight
- Complete the Comment Form
 - Leave it with us
or
 - Return to Susan Badtke at City Hall by October 7
(The presentation slides will be on the City website tomorrow)
- Attend the Public Hearing – January 17