



Memo

To: Anton Drive Steering Committee
From: Jason Valerius, Steve Tremlett
Subject: Draft Land Use and Development Concepts
Date: September 6, 2016

This memo accompanies a set of maps and development concepts that we will be discussing at the September 14 Steering Committee meeting. The following notes and narrative will assist your review prior to the meeting.

Conventional Zoning VS SmartCode

There are two ways that development can be regulated under Fitchburg ordinances. One method is conventional zoning, in which property is assigned a specific zoning “district” that defines the uses allowed and some basic dimensional limits such as minimum building setback, maximum height and maximum lot coverage. The other method of regulation is the City’s SmartCode, which could be applied to the areas that remain mostly undeveloped, including the Wingra Stone properties and the Fire Station block. Under SmartCode regulation, the design of blocks and the placement and size of buildings on those blocks is more tightly controlled to establish neighborhood character, but there is more flexibility regarding use. These draft materials consider both approaches.

Conventional Zoning (pages 1-9)

The first 9 pages of the attached PDF assumes a conventional approach, with land uses designated in the plan and illustrations of how development might occur under conventional zoning as allowed by those land use designations. There are two “conventional zoning” concepts shown, including a land use map and a development illustration for each. It is important to note that we are not asking you to pick between these – the final version will be crafted based on responses to the various ideas presented here. It is also important to note that while the land use designations in the final plan will become City policy and will inform what can be approved, the detailed concept plans are merely examples of how each area might develop within those land use designations – they will not have regulatory relevance, except to the extent that they inform revisions to the future land use map.

Page 2 – Land Use Representative Photos. These photos illustrate the variety and character of development envisioned within each land use category. Refer back to these images when you have questions about what a color on the map could allow.

Page 3 – Concept A Future Land Use Map. Note the blue arrows suggesting where new streets should connect to existing streets.

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Pages 4-5 – Concept A Development Concept. The concept suggests new uses and buildings on sites with a high likelihood of development or redevelopment. Note that in some portions of the planning area this concept shows the existing aerial photo, and some have a red or orange hatch. There is no known near-term interest in redevelopment for any of these sites, but the hatching indicates priorities for redevelopment among those sites, based on lower improvement values and/or design and character deficiencies.

Page 5 – Development Sections. These sections illustrate the spatial relationships between existing and potential development. At the west end of the Wingra Stone property. We anticipate some questions about how development as proposed in the quarry area will relate to existing, adjacent development, especially where the adjacent use is single family housing.

Page 7 – Concept B Future Land Use Map. Note the suggested extension of Roanoke Drive to Fitchrona Road.

Pages 8-9 – Concept B Development Concept.

SmartCode Zoning (pages 10-14)

If elected by the owner/developer during the development process, SmartCode zoning puts a high priority on urban form. It limits the size of blocks in the interest of establishing a more connected and flexible street grid, and it uses maximum setbacks to keep buildings close to those streets.

Sector Plan. This map simply identifies that the most appropriate sector as defined in the SmartCode is Sector G4 – Infill Growth Sector. It also identifies a preferred park location.

Regulating Plan. This map suggests the preferred location of development Transects, including a mix of T5 Urban Center Zone along McKee Road and Kapec Rd., and T4 General Urban Zone in other areas. A few notable development parameters in these Transects:

	T4	T5
Lot Width	18-96 ft.	18-180 ft.
Lot Coverage	70% max	90% max
Front Setback	6-18 ft	2-12 ft.
Building Height	1-3 stories	2-6 stories
Uses and Forms	Houses, Townhouses, Small Multifamily, Scattered Commercial	Shops, Townhouses, Large Multifamily, Offices, Predominantly Attached Buildings

Representative Images. These photos illustrate the variety and character of development envisioned within the T4 and T5 Transects. Refer back to these images when you have questions about what a color on the map could allow.

Note that this SmartCode Regulating Plan reflects neither Concept A nor Concept B, which were conceived as development as it might occur under conventional zoning.

Though there could be some differences in the final form and scale of development in the planning area due to which method of zoning is selected, the key constraint on development in this area is traffic. A review of our traffic analysis findings and recommendations follows, and will be integrated into the plan document.

Traffic and Street Infrastructure Changes

This plan is intended to establish guidelines for development that will ensure that as development occurs and additional vehicle trips are generated by that development, the four key local intersections in the planning area will continue to function at an acceptable level of service during the PM peak hour, which data show is the most

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congested period of the day in this area. Level of Service "D" is deemed acceptable, meaning wait times to get through a signalized intersection will rarely exceed 55 seconds.

To test for impacts on level of service at the four intersections we completed the following steps:

- 1) Utilized available traffic volume counts at the existing intersections and redistributed the traffic based on the WisDOT design changes that will be implemented beginning in the fall of 2016
- 2) Prepared trip distribution estimates for two land use concepts. Each concept was divided into 8 subareas and trips generated based on land use assumptions to approximate the percentage of trips that will likely flow into and out of the study area either through the Anton Drive/Williamsburg Way intersection, or through the Fitchrona Road/McKee Road intersection.
- 3) Modeled the impact of additional peak hour trips (using Synchro 9), to test for intersection function and Level of Service.

Based on that analysis we determined that Concepts A and B can be accommodated by the local street network, assuming several changes to the network as development proceeds. *The following guidelines are recommended:*

- Trips should be limited
- **The intersection of Fitchrona Road and McKee Road will need a second southbound to eastbound left turn lane before redeveloping the Wingra concrete plant area.**

The planning area intersection of greatest concern for congestion is Fitchrona Road and McKee Road. It is estimated that this intersection could accommodate approximately 750 total (inbound plus outbound) new PM peak hour trips in its current configuration, and an additional 400 total PM peak hour trips (1,150 total), including 650 total outbound trips (450 and an additional 200) if a second left turn lane is added to southbound Fitchrona Road. The additional traffic that could be accommodated by this second left turn lane corresponds roughly to the amount of development anticipated and traffic generation reserved for the parcels between the Fitchrona Road extension and Kapec Drive, including the current Wingra concrete plant, and it would be appropriate to tie such development to the addition of this left turn lane.
- **A portion of the right-of-way (ROW) for the extension of Fitchrona Road may need to be widened.**

The Fitchrona Road ROW as platted in 2016 is approximately 120 feet at the intersection with McKee Road and it narrows to approximately 60 feet north of that intersection. The development of the quarry and the concrete plant area may result in the need for turn lanes that the current ROW cannot accommodate. The current ROW is expected to function adequately without turn lanes until the concrete plant area redevelops. The need for additional ROW should be evaluated in conjunction with redevelopment planning for that site, and acquired from that site if needed. Further, it is preferable that Fitchrona Road have a minimum of 80 feet of ROW throughout the Wingra Stone area. If SmartCode is to be implemented in this area, this is a necessity, to allow for on-street parking.
- **The intersection of Kapec Road and Fitchrona Road may need changes as traffic increases.**

Excessive queue length and delay times may develop for drivers on Kapec Road, depending on the nature of development along Kapec Road. These delays could be alleviated by the addition

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of a second northbound lane that separates eastbound and westbound turns, and/or all-way stop control or a signal.

- **The intersection of Kapec/Nesbitt Road and McKee Road may need to be changed as traffic increases.**

The modeling shows some left turn queue lengths on McKee Road during the PM peak hour that exceed the planned turn lane storage bays on McKee. This includes both westbound traffic turning south onto Nesbitt Road, and eastbound traffic turning north onto Kapec. Drivers may choose other routes to avoid these queues, limiting their severity. If that natural redistribution of trips does not occur, these queues may back up traffic on McKee Road, resulting in the need for either closure of the left turn option, or reconstruction to increase storage bays for those left turns, or signalization (unlikely to be approved by WisDOT). If there are backups of westbound traffic that affect the function of the Verona Road interchange, changes will very likely be required by WisDOT.

- **The intersection of Anton Drive and King James Way may need changes as traffic increases, reserve space for a roundabout.**

The modeling of this intersection was prepared without traffic count data for the intersection, but estimates of future traffic flows suggest that the four-way stop may develop excessive queue lengths and delay times. The appropriate change cannot be predicted at this time, and could be as simple as changing to a 2-way stop if most of the traffic is on Anton Drive.

Alternatively, a signal or roundabout may be warranted. We recommend reserving space for a roundabout.

- **The roundabout as designed for Anton Drive and Williamsburg Way can accommodate the growth as illustrated in the development concepts, and more.**

With the enhancement to the Fitchrona/McKee intersection described above, a total of approximately 1,500 new peak hour trips can be accommodated within the entire study area. It is important to note that concepts A and B allot most of the development and new trips to the south end of the study area. Additional redevelopment along Anton Drive that would increase trip generation could still be accommodated, with the expectation that most of those trips will utilize Williamsburg Way for access to the study area. The modeling shows that there is additional capacity in the intersections of Williamsburg way with Anton Drive and Verona Road, beyond what is shown in Concept B.

- **Additional traffic impact analysis will be needed.**

An updated traffic impact analysis should be required for any project that would exceed the proposed PM peak hour trip generation of Concept B, and for any major redevelopment of the Wingra concrete plant area. It is important to note that the current analysis is based on the best data available from the City, the Wisconsin DOT and the Madison Area Transportation Planning Board describing current traffic and projected regional traffic growth and travel patterns. The background traffic volumes and patterns (meaning all traffic in the area other than the new traffic that would be generated as a result of new development in the study area) will likely differ from the various pre-construction projections, and this will affect the projected capacity of the study area intersections, especially Fitchrona Road/McKee Road, to accommodate growth within the study area. The City could allow development and redevelopment to proceed within

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the above-suggested limits, and then a new traffic impact analysis should be completed after the DOT Verona Road Stage 2 projects are constructed, utilizing new traffic counts.

CONVENTIONAL ZONING

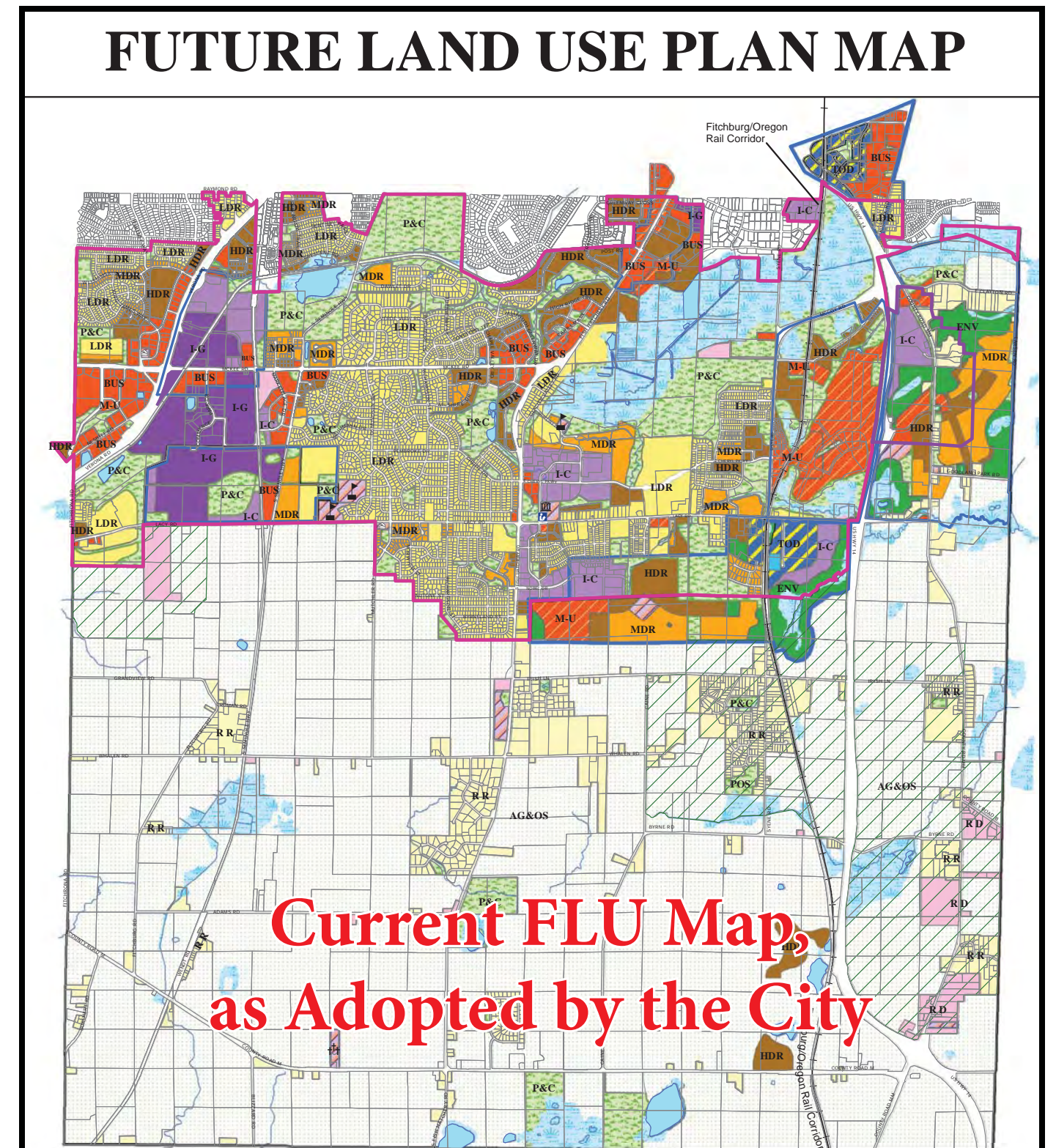
2 Future Land Use (FLU) -
Representative Photos

3-6 **Concept “A”**

- 3. FLU Map Revision
- 4. Conceptual Layout (south)
- 5. Conceptual Layout (north)
- 6. Sections

7-9 **Concept “B”**

- 7. FLU Map Revision
- 8. Conceptual Layout (south)
- 9. Conceptual Layout (north)



Med Density Residential



High Density Residential



Business



Mixed Use



CONCEPT "A"

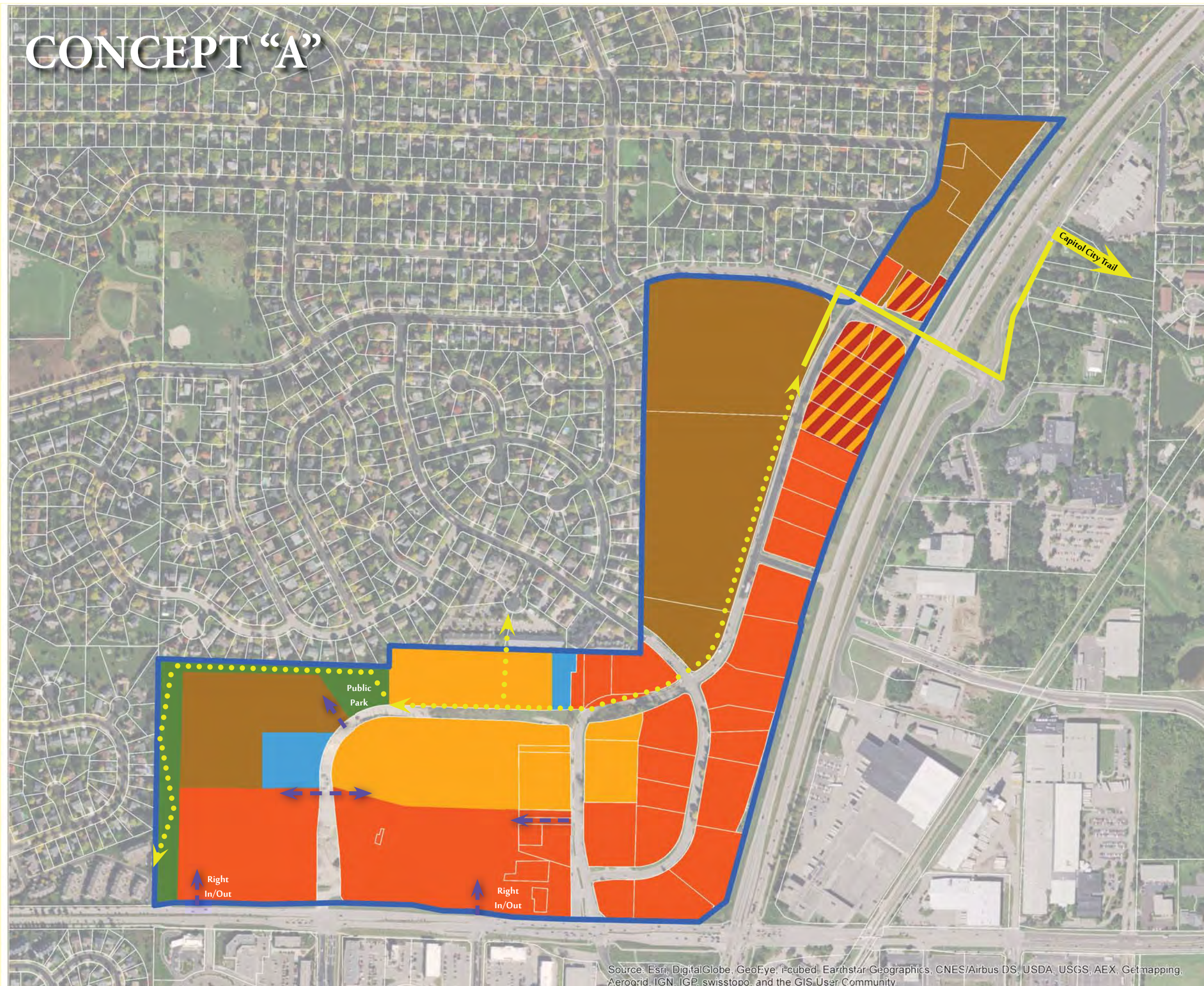
ANTON DRIVE REDEVELOPMENT PLAN

Future Land Use

Proposed by Anton Drive Redevelopment Plan


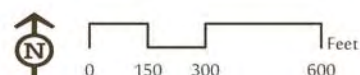
-  Planning Area
 -  Parcel
- Future Land Use**
-  Low Density Residential
 -  Medium Density Residential
 -  High Density Residential
 -  Mixed Use
 -  Business
 -  Industrial - Commercial
 -  Industrial - General
 -  Park & Conservancy
 -  Stormwater
- Potential Access/Trail**
-  Pedestrian Access/Trail
 -  Vehicle Access

DATA SOURCES:
 PARCELS AND ROAD NAMES PROVIDED BY DANE COUNTY.
 FUTURE LAND USE PROVIDED BY THE CITIES OF FITCHBURG AND MADISON.
 AERIAL IMAGERY PROVIDED BY ESRI.



Source: Esri, DigitalGlobe, GeoEye, i-cubed, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community

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CONCEPT "A"

This is a conceptual illustration of potential development in the Anton Drive planning area, provided to support discussion about how this area could develop and the drafting of land use and urban design policy for the area. While the final plan document will include a conceptual plan in this format, showing specific streets, parcels and buildings, all participants should understand that development will be proposed by property owners, and subject to City review and approval using this plan as guidance for that approval.



CONCEPT "A"

This is a conceptual illustration of potential development in the Anton Drive planning area, provided to support discussion about how this area could develop and the drafting of land use and urban design policy for the area. While the final plan document will include a conceptual plan in this format, showing specific streets, parcels and buildings, all participants should understand that development will be proposed by property owners, and subject to City review and approval using this plan as guidance for that approval.

FIRST PRIORITY REDEVELOPMENT

SECOND PRIORITY REDEVELOPMENT

FIRST PRIORITY REDEVELOPMENT

SECOND PRIORITY REDEVELOPMENT



SW FACILITY

3-Story OFFICE
24,625 SF

4-Story MULTIFAMILY
36 UNITS

4-Story MULTIFAMILY
27 UNITS

3-Story OFFICE
17,375 SF

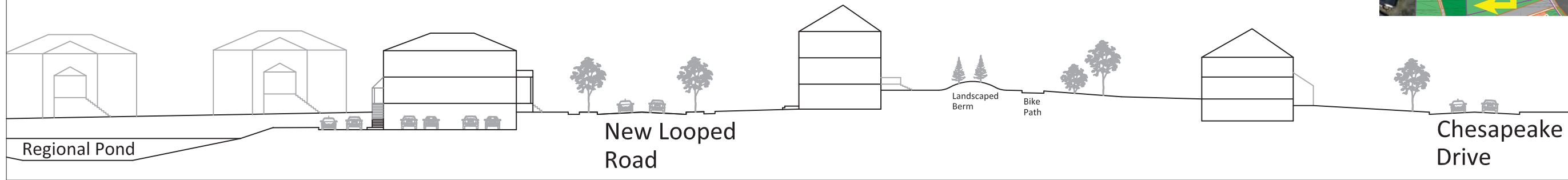
2-Story OFFICE
18,300 SF

4-Story OFFICE
35,000 SF

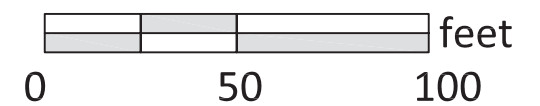
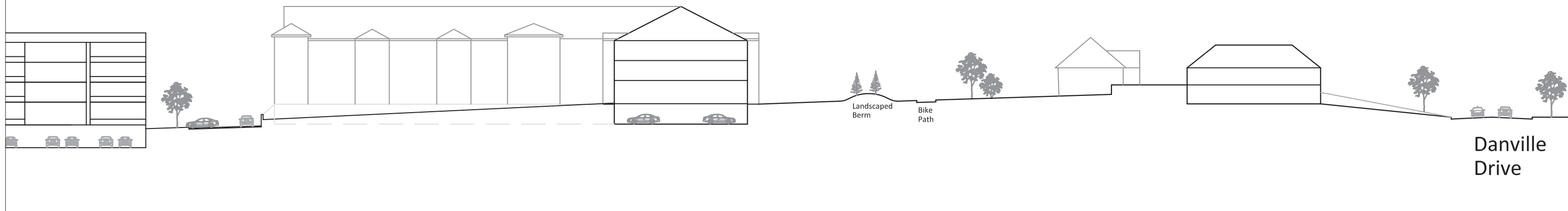
3-Story MIXED COM
46,000 SF

CONCEPT "A"

Section 1



Section 2




CONCEPT "B"

ANTON DRIVE REDEVELOPMENT PLAN


Future Land Use

Proposed by Anton Drive
Redevelopment Plan


 Planning Area

 Parcel


Future Land Use


 Low Density Residential


 Medium Density Residential

 High Density Residential

 Mixed Use

 Business

 Industrial - Commercial

 Industrial - General

 Park & Conservancy

 Stormwater

Potential Access/Trail

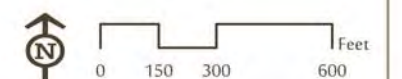
 Pedestrian Access/Trail

 Vehicle Access

DATA SOURCES:
PARCELS AND ROAD NAMES PROVIDED BY DANE COUNTY.
FUTURE LAND USE PROVIDED BY THE CITIES OF FITCHBURG AND MADISON.
AERIAL IMAGERY PROVIDED BY ESRI.

CITY OF FITCHBURG
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MSA
PROFESSIONAL SERVICES

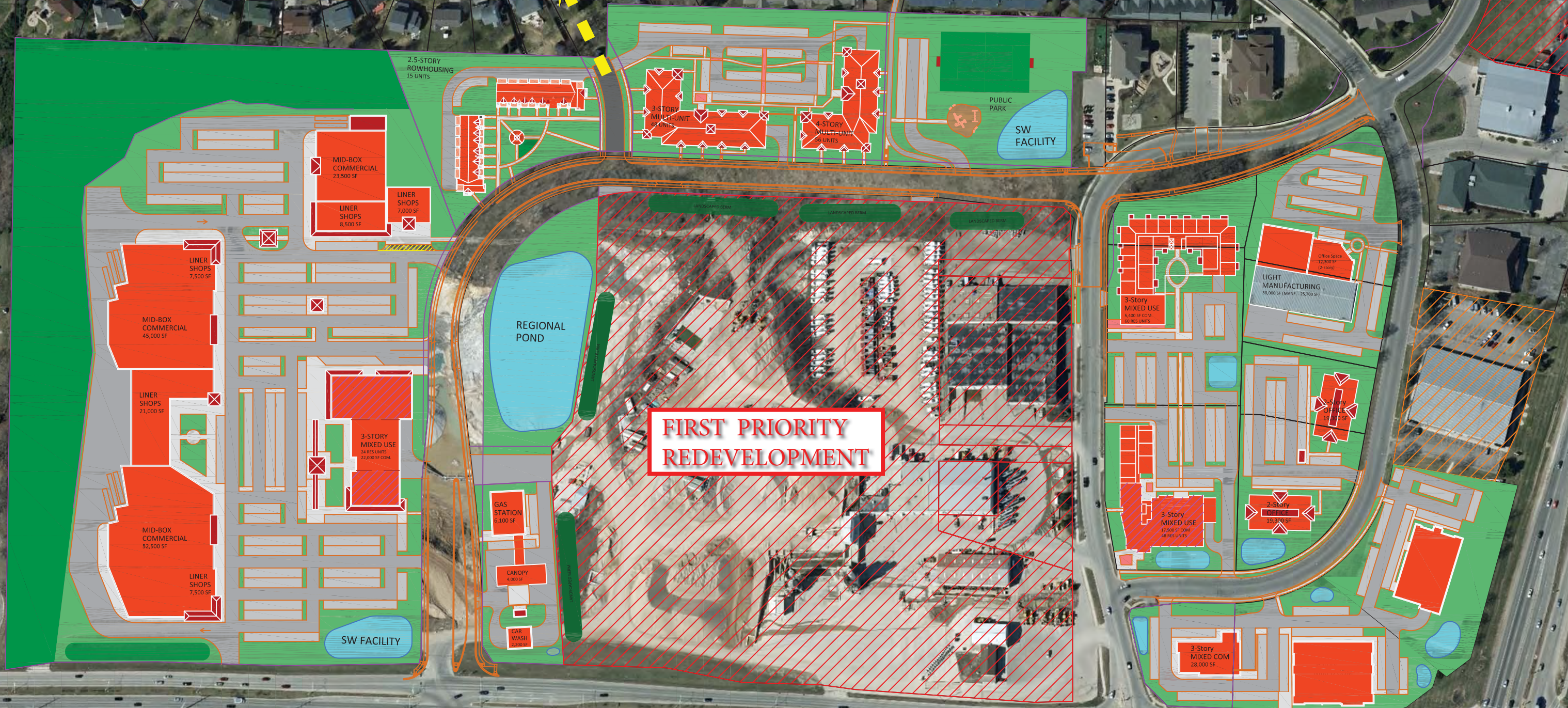


Source: Esri, DigitalGlobe, GeoEye, i-cubed, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community

CONCEPT "B"

This is a conceptual illustration of potential development in the Anton Drive planning area, provided to support discussion about how this area could develop and the drafting of land use and urban design policy for the area. While the final plan document will include a conceptual plan in this format, showing specific streets, parcels and buildings, all participants should understand that development will be proposed by property owners, and subject to City review and approval using this plan as guidance for that approval.

POTENTIAL ROAD CONNECTION

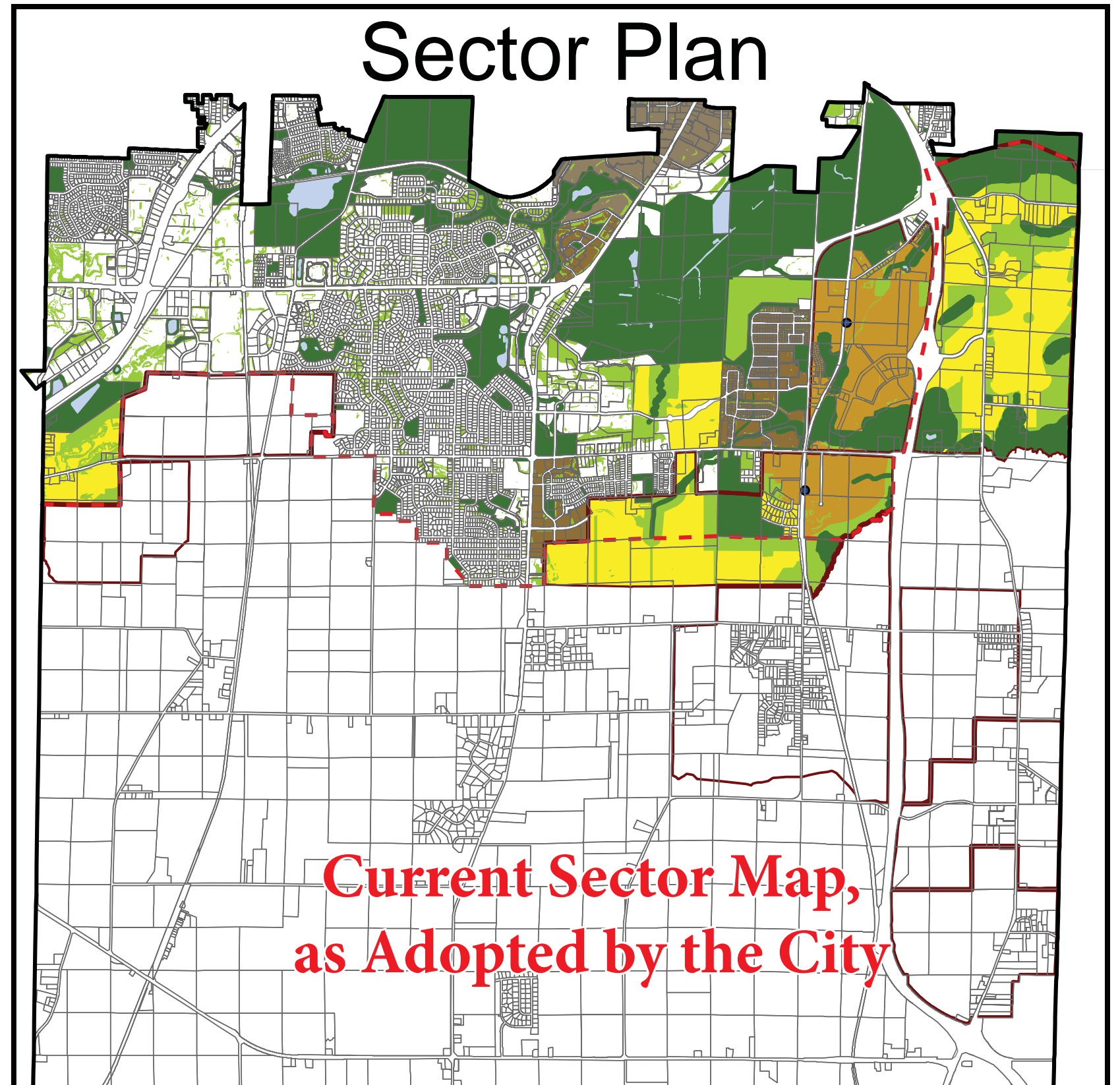


FIRST PRIORITY REDEVELOPMENT

0 100 200 FEET

SMART CODE ZONING

- 12-13 **Concept “A”**
 - 12. Sector Plan Revision
 - 13. Regulating Plan
- 14 **Sector Plan - Representative Photos**






This plan proposes that a portion of the Anton Drive Planning Area should be designated as G4 – Infill Growth Sector. If adopted as part of this plan, the City’s Sector Plan will be amended to reflect this designation, enabling owners of these lands the option of utilizing SmartCode Zoning.

ANTON DRIVE REDEVELOPMENT PLAN

SECTOR PLAN

Proposed by Anton Drive
Redevelopment Plan

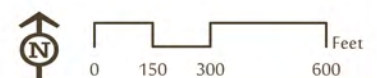
-  Municipal Boundary
-  Planning Area
-  Parcel

Open Space and Growth Sectors

-  01 - Preserved Open Sector
-  02 - Reserved Open Sector
-  G2 - Controlled Growth Sector
-  G3 - Intended Growth Sector
-  G4 - Infill Growth Sector
-  G5 - Repair Sector

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Source: Esri, DigitalGlobe, GeoEye, i-cubed, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community

This is a sample regulating plan, an example of how the lands designated for optional SmartCode zoning could be assigned to transects within the G4 Sector. While the final plan document will include a sample regulating plan in this format, showing specific streets, blocks and transects, all participants should understand that development will be proposed by property owners, and subject to City review and approval. Those proposals may or may not utilize SmartCode zoning within this area, and, if utilizing SmartCode, the regulating plan may differ from this sample, with a different configuration of T3, T4 and T5 transects as permitted in the G4 Sector. Note that if SmartCode Zoning is proposed, the land use designations that guide conventional zoning approvals will not apply.

ANTON DRIVE REDEVELOPMENT PLAN

REGULATING PLAN

Proposed by Anton Drive
Redevelopment Plan

-  Municipal Boundary
-  Planning Area
-  Parcel
-  T1 Natural Zone
-  T4 General Urban Zone
-  T5 Urban Center Zone
-  Stormwater Management Facility
-  Civic Zone
-  Playground

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**T4 -
General
Urban**



**T5 -
Urban
Center**

